

Fourteenth Schedule

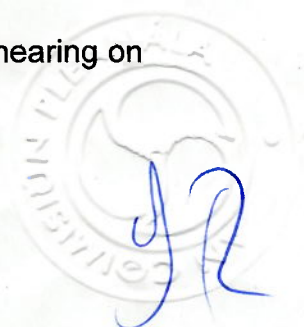
Conditions Imposed by An Coimisiún Pleanála

1. The proposed development shall be carried out and completed in accordance with the plans and particulars lodged with the application, as updated and modified by the following:
 - (a) Schedule of Errata, submitted at the oral hearing on the 27th day of March 2024,
 - (b) Schedule of Updates, submitted at the oral hearing on the 27th day of March 2024, and
 - (c) Per- and Poly-Fluoroalkylated Substances (PFAS) Management Strategy for Dublin Airport, submitted to the Commission on the 31st day of January 2025,

except as may otherwise be required in order to comply with the following conditions.

Reason: In the interest of clarity.

2. The following modifications are made to the Railway Order:
 - (a) the text of the Railway Order shall be updated to reflect the revised and updated version submitted to the Commission at the oral hearing on the 26th day of March 2024,
 - (b) Schedules 2 - 10 (i.e. the Book of Reference) to the Railway Order shall be updated to reflect the Schedules submitted to the Commission on the 12th day of June 2024,
 - (c) the following agreements reached by the applicant shall be included in the Thirteenth Schedule of the Railway Order:
 - (i) Agreement with Dublin City Council, submitted at the oral hearing on 26th day of February 2024.



- (ii) Agreement with Fingal County Council, submitted at the oral hearing on 4th day of March 2024.
- (iii) Agreement with the Office of Public Works, submitted at the oral hearing on 26th day of February 2024,
- (iv) Agreement with DAA, submitted at the oral hearing on 26th day of February 2024,
- (d) the conditions hereby attached to the grant of the Railway Order shall be included in the Fourteenth Schedule of the Railway Order,
- (e) the Fifteenth Schedule shall be omitted,
- (f) the Commission's reasoned conclusion on Environmental Impact Assessment, hereby attached to the grant of the Railway Order, shall be included in an amended Fifteenth Schedule of the Railway Order, entitled 'Reasoned Conclusion', and
- (g) the Sixteenth Schedule of the Railway Order shall be entitled 'Schedule of Costs'.

Reason: In the interests of clarity and the proper planning and sustainable of the area.

3. All of the mitigation measures, environmental commitments and monitoring measures identified in the following documents shall be implemented in full as part of the proposed scheme:

- (a) the Environmental Impact Assessment Report,
- (b) the Natura Impact Statement, and
- (c) the Schedule of Additional Environmental Commitments, submitted at the oral hearing on the 27th day of March 2024;

except as may otherwise be required in order to comply with the following conditions.

A consolidated Schedule of Mitigation and Monitoring Measures, derived from parts (a) to (c) of this condition, shall be prepared and submitted to Dublin City



Council, Fingal County Council, the Minister for Transport and made available on a publicly accessible website, prior to the commencement of construction.

Reason: In the interests of clarity and the protection of the environment during the construction and operational phases of the development.

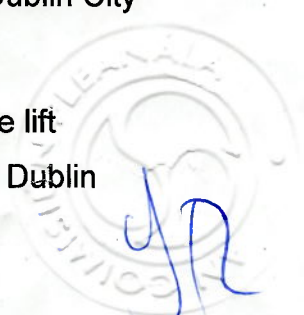
4. Station designs shall be amended as follows:

(a) All stations:

- (i) Where skylights are proposed, they shall have a maximum height of 1.5 metres above ground level. Prior to the commencement of development, amended plans, sections and elevations indicating compliance with this condition shall be submitted for the written agreement of Dublin City Council and Fingal County Council, as relevant.
- (ii) No signage or advertising, other than MetroLink related operational signage, shall be affixed to the glazed facades of the station entrances.
- (iii) Prior to their installation, details of the final design and positioning of the hostile vehicle management bollards at each station shall be submitted for the written agreement of Dublin City Council and Fingal County Council, as relevant. The bollards at St. Stephen's Green Station shall be of matching stone material and appearance to existing bollards that surround the perimeter of the park.

(b) Mater Station:

- (i) The design of the station entrance canopy shall be revised to incorporate reduced metal banding and a commensurate increase in glazing. Prior to the commencement of development, amended plans, sections and elevations indicating compliance with this condition shall be submitted for the written agreement of Dublin City Council.
- (ii) The design of the combined passenger/Dublin Fire Brigade lift structure to the south-west of the station entrance and the Dublin



Fire Brigade lift structure to the west of St. Joseph's Church shall be revised to comprise glazed lift shafts. Prior to the commencement of development, amended plans, sections and elevations indicating compliance with this condition shall be submitted for the written agreement of Dublin City Council.

(iii) The proposed skylights shall be omitted.

(c) O'Connell Street Station:

(i) The O'Connell Street station entrance shall be as shown in the document 'O'Connell Street Station' and the updated drawings submitted at the oral hearing on the 19th day of March 2024 and the 25th day of March 2024, respectively.

(ii) In the event that the commercial development, known as Dublin Central, which was permitted by the Commission under ABP-318316-23 does not proceed, the Moore Lane station entrance shall be as shown in the document 'O'Connell Street Station' submitted at the oral hearing on the 19th day of March 2024, and shall incorporate a brick treatment to the entrance and planting in front of the site hoarding along Moore Lane. This shall be maintained by the developer until such time as the surrounding lands are developed.

(d) Charlemont Station: The pedestrian stairs and lift to the front of the Carroll's Building shall be revised such that it is set away from and does not overhang the entrance canopy to the existing building. Prior to the commencement of development, amended plans, sections and elevations indicating compliance with this condition shall be submitted for the written agreement of Dublin City Council.

Reason: In the interests of visual amenity and the preservation of features of heritage interest.



5. Prior to the commencement of development, a finalised Construction Environmental Management Plan (CEMP), including a Scheme Traffic Management Plan, shall be prepared in consultation with Dublin City Council and Fingal County Council. The CEMP shall incorporate all mitigation measures as set out in the Environmental Impact Assessment Report, the Natura Impact Statement and the conditions set out herein and shall include details and schedules of monitoring supervision and reporting to the planning authorities. In finalising the CEMP and the Scheme Traffic Management Plan the developer shall liaise with National Parks and Wildlife Service, Inland Fisheries Ireland, the National Transport Authority, Waterways Ireland, Córas Iompair Éireann and its constituent companies, as appropriate. The CEMP and Scheme Traffic Management Plan shall be updated on a regular basis, to be agreed with Dublin City Council and Fingal County Council as appropriate, during the construction phase in consultation with the abovementioned bodies and the Project Construction Traffic Forum.

Reason: To protect amenities, public health and safety.

6. Dust monitoring locations shall be agreed with the local authorities and shall include a dust monitoring location as close to the junction of Moore Street and O'Rahilly Parade as feasible.

Reason: In the interest of the amenity of the area.

7. The measures outlined in the document 'St. Stephen's Green Park Draft Schedule of Railway Order Conditions', submitted by the applicant at the oral hearing on the 26th day of February 2024, shall be implemented in full except as may otherwise be required in order to comply with the conditions attached to this Railway Order.

Reason: In the interests of visual amenity and the preservation of features of heritage interest.



8. A construction method statement shall be submitted to Dublin City Council, which outlines proposals for:
- (a) the protection of street trees on O'Connell Street during station construction,
 - (b) the removal of scaffolding along O'Connell Street following internal propping of retained facades, and
 - (c) the removal of construction hoardings on O'Connell Street following the completion of MetroLink construction.

Reason: In the interests of visual amenity and the preservation of features of heritage interest.

9. The proposed construction site hoardings shall comply with the following requirements:
- (a) Site hoardings at the following locations, where they are 4 metres or greater in height and directly address adjacent housing, shall incorporate a transparent material for the upper 2 metres: Glasnevin, Mater and Charlemont.
 - (b) The developer shall prepare and submit a scheme of painted and/or artwork finishes for construction site hoardings, which shall include the incorporation of locally relevant art, for the written agreement of Fingal County Council or Dublin City Council, as appropriate. The approved scheme shall thereafter be implemented at each site within 6 months after erection of site hoardings and shall be maintained for the duration of the construction phase to the satisfaction of Fingal County Council or Dublin City Council, as appropriate.
 - (c) Where construction site hoardings obstruct visibility of businesses, provision shall be made for temporary signage attached to the hoardings for those businesses.

Reason: In the interests of residential and visual amenity.



10. (a) Proposed tree planting at Albert College Park shall include mature planting, in accordance with a scheme to be submitted for the written agreement of Dublin City Council prior to commencement of construction at Albert College Park.
- (b) For the avoidance of doubt, no felling of street trees at St. Mobhi Road shall take place.

Reason: In the interest of visual amenity.

11. Prior to commencement of construction works at Balheary Park, the developer shall make available an alternative training facility for Fingallians GAA, which shall be maintained in such use until the existing facility at Balheary Park is reinstated.

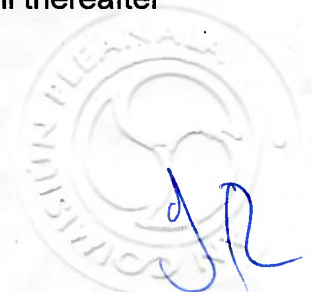
Reason: In the interest of orderly development.

12. Prior to the removal of car parking spaces at The Court apartments, Dalcassian Downs, Glasnevin, the developer shall make available an equal number of alternative parking spaces for affected residents, which shall be maintained until such time as new parking spaces are provided following the completion of construction of Glasnevin Station.

Reason: In the interest of residential amenity.

13. (a) Prior to the commencement of development, the developer shall submit proposals for the written agreement of Fingal County Council for the reconstruction of removed sections of the Balheary Demesne boundary wall along the newly created demesne boundary.
- (b) Prior to the commencement of any construction in the vicinity of Balheary Bridge, a construction hoarding shall be erected a minimum of 10 metres away from the bridge. All construction plant and machinery shall thereafter be kept outside of this exclusion zone.

Reason: In order to preserve features of heritage importance.



14. Operational stage drainage arrangements, including the attenuation and disposal of surface water, shall comply with the requirements of Fingal County and Dublin City Council, as appropriate.

Reason: In the interests of environmental protection and public health.

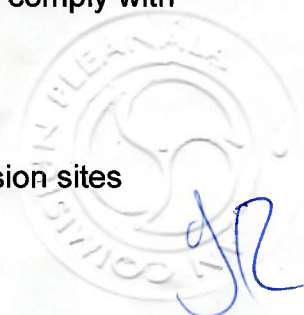
15. Prior to the commencement of development, the developer shall enter into Connection Agreements with Uisce Éireann to provide for service connections to the public water supply and/or wastewater collection network for both the construction and operational phases.

Reason: In the interest of public health and to ensure adequate water/wastewater facilities.

16. All construction phase water requiring disposal shall be disposed to the public sewer under agreement and not to any surface water body.

Reason: In the interests of clarity and protection of the environment.

17. (a) Mitigation measures contained within the Natura Impact Statement that are intended to protect against the potential for discharge of surface waters from construction sites shall accord with advice contained with Inland Fisheries Ireland guidance document 'Guidelines on Protection of Fisheries During Construction Works in and Adjacent to Waters' (2016). Additional drawings and/or construction method statements outlining compliance with the Inland Fisheries Ireland Guidelines shall be submitted to Fingal County Council and Dublin City Council, as appropriate, for agreement.
- (b) The proposed in-water installation at the Broadmeadow River, required as part of viaduct construction, shall be designed and installed under the supervision of the project Ecological Clerk of Works and shall comply with the abovementioned Inland Fisheries Ireland Guidelines.
- (c) The developer shall undertake water quality monitoring at the Broadmeadow River and at all culvert and watercourse diversion sites

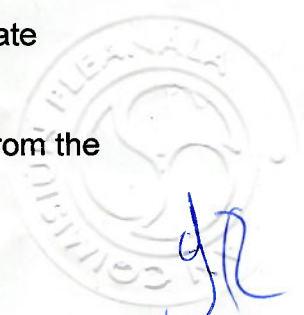


before and after completion of works, to confirm that there is no pollutant/contaminant in the water prior to removal of installations that are in place to protect receiving water quality.

- (d) The developer shall submit a Sediment Erosion and Pollution Control Plan to Fingal County Council and Dublin City Council for their written agreement, prior to the commencement of construction.

Reason: In the interest of environmental protection and in order to protect water quality within receiving surface waters.

- 18. (a) The Expert Panel described in the document 'Ground Movement Monitoring Information Paper', submitted at the oral hearing on 19th day of February 2024, shall be engaged prior to procurement of the Independent Monitoring Engineer and the main civil works contractor. The Expert Panel shall verify the scope of the Independent Monitoring Engineer and the contract requirements for building damage risk assessments.
- (b) The Expert Panel shall have a verifying and sign off' role rather than the advisory role stated by the applicant in the document referenced in part (a) above.
- (c) The developer shall procure the Main Civil Works and implement the work to respect the commitments made in the document 'Working Draft / Process for the Oversight and Implementation of Phase 3 Assessments' submitted at the oral hearing on the 6th day of March 2024 and the Schedule of Additional Environmental Commitments submitted at the oral hearing on the 27th day of March 2024.
- (d) The developer shall ensure that a 'Plan for the Advancement of the Tunnel' (PAT) is developed for each section of tunnel and each 'Plan for the Advancement of the Tunnel' shall be reviewed and signed off by the Independent Monitoring Engineer and Expert Panel.
- (e) The Tunnel Boring Machine specifications shall include appropriate software and hardware that shall provide the developer and the Independent Monitoring Engineer with access to real time data from the

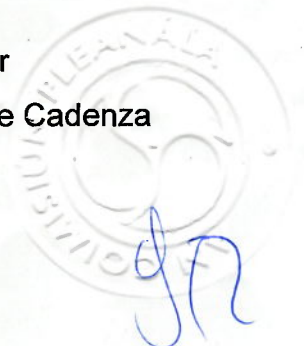


tunnelling operations. The Tunnel Boring Machine specifications shall also provide for integration with other monitoring, such as settlement and displacement, in real time.

- (f) The developer shall carry out baseline monitoring for at least one year prior to commencement of piling or diaphragm wall construction to establish baseline movement of representative, heritage and 'special' buildings (as defined in section 5.4.11.2 of the Environmental Impact Assessment Report) and all buildings anticipated to require a Phase 3 Building Damage Assessment.
- (g) The developer shall put in place Interferometric Synthetic Aperture Radar monitoring of the alignment commencing at the start of the baseline monitoring period, continuing through construction and up to the completion of the close-out monitoring period. The Interferometric Synthetic Aperture Radar data shall cover a zone approximately 100 metres either side of the tunnel centre line.
- (h) The ventilation and evacuation tunnels at Dublin Airport shall be constructed by means of a closed-face Tunnel Boring Machine.

Reason: In the interest of environmental protection and to safeguard the amenity of property in the vicinity.

- 19. (a) The developer shall put in place suitable measures to ensure that building damage does not exceed Category 1, as defined in Table 4-4 of the Building Damage Report contained in Appendix A5.17 of the Environmental Impact Assessment Report.
- (b) The Expert Panel shall review and sign off on Phase 3 assessments and the measures to be undertaken for sensitive locations of the alignment. These shall include:
 - (i) the area around Charlemont Station,
 - (ii) sensitive buildings, such as those with deep basements or settlement sensitive facades (including AerCap House, the Cadenza

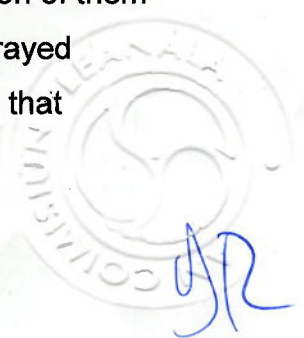


Building and the 2 Grand Parade development at Charlemont Station),

- (iii) buildings adjacent to tunnel break-in / break-out zones from the station boxes, and
- (i) Protected Structures and other 'special' buildings (as defined in section 5.4.11.2 of the Environmental Impact Assessment Report).
- (c) Prior to the commencement of construction, the developer shall undertake a further review of both the Fingal County Council and Dublin City Council Records of Protected Structures. Any additional Protected Structures with the potential zone of influence that had not previously been identified as requiring building damage assessment shall be subject to Phase 3 assessment.

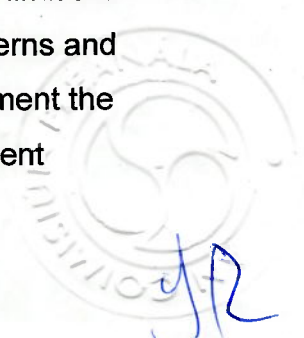
Reason: In the interest of the protection of architectural heritage and to mitigate the risk of building damage during construction.

20. (a) Blasting shall not be carried out for the Sprayed Concrete Lined works, including Albert College Park Intervention Shaft and tunnels, Sprayed Concrete Lined pumping sumps, and Sprayed Concrete Lined works at Dublin Airport, in addition to the commitment to not use blasting for the Charlemont Intervention Tunnel.
- (b) Blasting shall not be carried out for the construction of the following deep stations: Charlemont Station, St. Stephens Green Station, Tara Station, O'Connell Street Station and Mater Station.
- (c) At other locations, the developer shall ensure that the effects from blasting do not extend beyond the zone covered by the Property Owners Protection Scheme.
- (d) The Expert Panel shall review the scope of the additional detailed design geotechnical investigations and the adequacy of the interpretation of them at sensitive locations. This shall include the locations where Sprayed Concrete Lined works are to be carried out and where buildings that require a Phase 3 Building Damage Assessment are located.



Reason: In the interest of the protection of architectural heritage and to mitigate the risk of building damage during construction.

21. (a) The developer shall take such measures for controlling groundwater dewatering from the deep station construction as are necessary to ensure that no groundwater lowering shall occur outside the station boxes as stated in the document 'Dewatering Assessment in Station Boxes and SCL Tunnels (Version 2)' submitted at the oral hearing on 6th day of March 2024. This requirement shall be applied to all deep stations.
- (b) The developer shall take such measures for controlling the groundwater dewatering from the Sprayed Concrete Lining works (e.g. Charlemont Intervention Tunnel, Albert College Park Intervention Shaft and tunnels and area AZ4 pumping sumps) as are necessary to ensure that no groundwater lowering occurs, as stated in the document referenced in part (a) of this condition.
- (c) The definition of "no groundwater lowering" shall be that no groundwater lowering occurs outside the normal seasonal variation of groundwater levels as determined by at least 1 year of pre-excavation monitoring.
- (d) The Expert Panel shall review and once satisfied, shall sign off on the groundwater control measures to be adopted by the contractor prior to implementation.
- (e) The Expert Panel shall follow the execution and monitoring of the groundwater control works and shall have the power to instruct the developer to carry out any additional measures that the Expert Panel consider necessary to ensure that no groundwater lowering occurs outside the station boxes and Sprayed Concrete Lining works.
- (f) The developer shall implement groundwater control measures to limit the abstraction at Dublin Airport Station and associated tunnels, caverns and shafts to a maximum of 32 cubic metres per day and shall implement the measures set out in the Per- and Poly-Fluoroalkylated Management

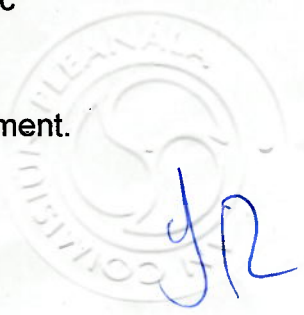


Strategy for Dublin Airport, submitted to the Commission on the 31st day of January 2025.

Reason: In the interest of environmental protection and to safeguard the amenity of property in the vicinity.

22. (a) The document 'Outline Guidance Note for Developers' submitted at the oral hearing on the 27th day of March 2024 shall be updated to clearly identify the level at which the allowable 75 kilonewtons per square metre load is applied and it shall be 'signed off' by the Expert Panel, included in the design and build tender, submitted to Dublin City Council and Fingal County Council and made available on a publicly accessible website. In the interests of clarity, the MetroLink shall not limit future developments to a maximum surcharge load of 75 kilonewtons per square metre at the base of their foundations, as stated at section 4.2.3.1 'Construction Stage' of 'MetroLink Railway Order 2nd Statutory Consultation – Response to Submissions Received' as received by An Coimisiún Pleanála on the 9th day of December 2024
- (b) Suitable provisions shall be included in the design to ensure that the tunnel has suitable robustness to cope with the anticipated loading from adjacent developments and not unduly constrain future development.
- (c) Prior to the coming into operation of the MetroLink, the developer shall publish on the project website and submit to Dublin City Council and Fingal County Council, an updated Guidance Note for Developers which will identify exclusion and protection zones around stations and tunnels in accordance with the document referenced in part (a) of this condition, but which shall omit the Limits of Deviation.
- (d) The updated Guidance Note for Developers shall incorporate pre-approved guidance criteria for minor works that have no reasonable likelihood of impacting on MetroLink infrastructure (e.g. domestic extensions, boundary walls, shallow excavations).

Reason: In the interests of proper planning and sustainable development.



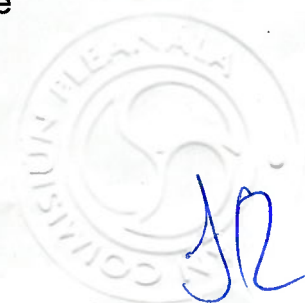
23. The construction working hours shall be as set out in the document 'Update to Chapter 5 MetroLink Construction Phase: Working Hours', submitted at the oral hearing on 29th day of February 2024, with the following exception:

- (a) The Sprayed Concrete Lining works for the Intervention Tunnels at Charlemont, Albert College Park and the pumping sumps in area AZ4 shall be undertaken on a 24 hours per day, 5.5 day per week basis, with no weekend working later than 13:00 on Saturdays with no work on Sundays and public holidays.

The Stakeholder Communication Plan shall include measures for the advance communication of working hours, including the need for night-time working activities as and when they arise, to residents and businesses in the vicinity of each works site.

Reason: To safeguard the amenity of property in the vicinity.

24. (a) The 'Airborne Noise & Groundborne Noise Mitigation Policy' contained in Appendix A14.6 of the Environmental Impact Assessment Report shall be amended to remove reference to a maximum 4-week period for temporary rehousing. The duration of rehousing shall be tied to the duration of the activity or activities that are the cause of the relevant noise trigger values for rehousing being exceeded.
- (b) For noise control from fixed installations, in addition to the measures proposed by the applicant in the document 'Noise Control from Fixed Installations for MetroLink', submitted at the oral hearing on the 5th day of March 2024, specific thresholds for the frequencies between 10 and 160 hertz shall be set in accordance with the NANR34 assessment referred to in the British Standard BS4142.
- (c) The baseline noise surveys shall be re-run prior to the commencement of construction and the results shall be used in updated noise assessments and in the updating and further development of the CEMP and the Construction Noise and Vibration Management Plans.



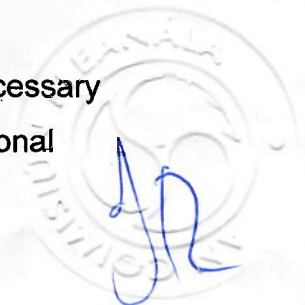
- (d) Noise modelling shall be carried out in to provide three dimensional (3D) 'Predicted Noise Maps' which shall be used for the preparation of the Construction Noise and Vibration Management Plans and as a basis shall use the specific noise emissions for the plant to be used and their location on site.
- (e) The three-dimensional noise modelling shall be used to communicate the noise impact from upcoming activities to affected receptors and shall be published by the developer through a suitable web portal.
- (f) The results of airborne and groundborne noise and vibration monitoring shall be published on a daily basis by the developer on a suitable web portal.
- (g) The actual groundborne vibration propagation and the coupling between the tunnel and ground and ground/buildings shall be determined by placing a seismic vibrator in the completed tunnel prior to trackbed construction. This shall be carried out at a number of representative locations along the alignment, such as in the vicinity of Trinity College Dublin, Mater Hospital and representative heritage buildings in area AZ4. The measurements shall use a range of relevant frequencies and the response at ground surface and selected buildings measured. This approach shall be used to calibrate the models used for the predictions of groundborne noise and vibrations and shall be used to confirm that the selected trackbed solution will provide the required result in terms of groundborne vibrations during the operational phase.
- (h) The reduction of the operational phase groundborne noise limit, as proposed by the applicant in the document 'Additional Groundborne Noise Commitment' submitted at the oral hearing on the 5th day of March 2024, which commits to a maximum groundborne noise levels in any lawfully occupied residential dwellings, measured near the centre of any noise-sensitive room, of 35 dBL_{Amax}, shall also apply to selected heritage buildings and commercial office buildings and the developer shall agree with the Expert Panel which buildings this will be applicable to.



- (i) Noise and vibrations monitoring shall be carried out by the Independent Monitoring Engineer and overseen by the Expert Panel.
- (j) Trigger Action Plans shall be developed for those buildings that are sensitive to noise and vibration during passage of the Tunnel Boring Machine that do not currently have a commitment for a Trigger Action Plan to be produced or for noise and vibration monitoring to be carried out during the passage of the Tunnel Boring Machine. The developer shall agree with the Expert Panel which buildings this is applicable to, but it shall include the Cadenza Building and AerCap House.
- (k) The bentonite recycling plants at the Northwood Portal site, Collins Avenue, Mater, St. Stephen's Green and Charlemont Station sites shall be contained within acoustic structures, details of which shall be submitted to Dublin City Council for agreement prior to the commencement of development.

Reason: In order to protect the amenities of property in the vicinity.

25. The developer shall engage a suitably qualified archaeologist to monitor (licensed under the National Monuments Acts) all site clearance works, topsoil stripping, groundworks, dredging and/or the implementation of agreed preservation in-situ measures associated with the development. The use of appropriate machinery to ensure the preservation and recording of any surviving archaeological remains shall be necessary. Should archaeological remains be identified during the course of archaeological monitoring, all works shall cease in the area of archaeological interest pending a decision of the planning authority, in consultation with the National Monuments Service, regarding appropriate mitigation. The developer shall facilitate the archaeologist in recording any remains identified. Any further archaeological mitigation requirements specified by the planning authority, following consultation with the National Monuments Service, shall be complied with by the developer. Following the completion of all archaeological work on site and any necessary post-excavation specialist analysis, the planning authority and the National



Monuments Service shall be furnished with a final archaeological report describing the results of the monitoring and any subsequent required archaeological investigative work/excavation required. All resulting and associated archaeological costs shall be borne by the developer.

Reason: To ensure the continued preservation of places, caves, sites, features or other objects of archaeological interest.

26. The developer shall ensure that Conservation Architects with demonstrable expertise and experience in urban design in historic contexts, are engaged as part of the proposed development to identify, confirm and develop the record of historic fabric and inform appropriate levels of intervention and protection to protected structures and works within Architectural Conservation Areas.

Reason: In the interest of architectural heritage protection.

27. The existing elevated footbridges over the R132 regional road shall not be demolished until such time as the permitted R132 Connectivity Project (ABP-310145-21) is complete and alternative at-grade signalised pedestrian crossing points have been provided.

Reason: In the interests of road and pedestrian safety.

28. Prior to the installation of the temporary bridge over the Royal Canal, the developer shall undertake a road safety audit. All recommendations from the road safety audit shall be incorporated into the final design prior to installation.

Reason: In the interest of road and pedestrian safety.



29. The developer shall undertake the construction of Charlemont Station in a manner that ensures that Dartmouth Road shall be re-opened to local access traffic as soon as is feasible and the duration of the full closure of Dartmouth Road to local access traffic shall not exceed 30 months.

Reason: In order to protect the amenities of property in the vicinity.

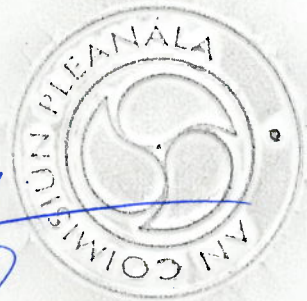
30. (a) Demand management measures shall be put in place for the operation of the Estuary Park and Ride Facility to ensure its strategic function is protected and to discourage or prohibit long-term car parking. Details of the demand management measures shall be submitted to Fingal County Council for agreement prior to the coming into operation of the car park.
- (b) A survey of cycle parking usage and demand in the opening year and 5 years after the opening year shall be undertaken and submitted to Dublin City Council, Fingal County Council and the National Transport Authority. As part of this study, a review of bicycle theft at MetroLink stations shall be undertaken to determine if further secure cycle parking or other security measures are required.
- (c) The applicant shall carry out a review of footpath capacity within 12 months of coming into operation of the MetroLink stations and a further review shall be carried out between year 5 and year 6. The areas of footpath to be subject of the reviews shall be agreed in writing with Fingal County Council and Dublin City Council, as appropriate and the findings of the reviews shall be furnished to Fingal County Council and Dublin City Council, as appropriate.

Reason: In the interest of sustainable transport.



31. The green roofs proposed to a number of the stations shall be adequately protected from damage until established. Any areas of damaged grass to the roofs shall be replaced within the next planting season.

Reason: In the interest of visual amenity.



Tom Rabbette

Planning Commissioner of An Coimisiún

**Pleanála duly authorised to authenticate
the seal of the Commission.**

Dated this ^{30th} day of *Sept.* 2025